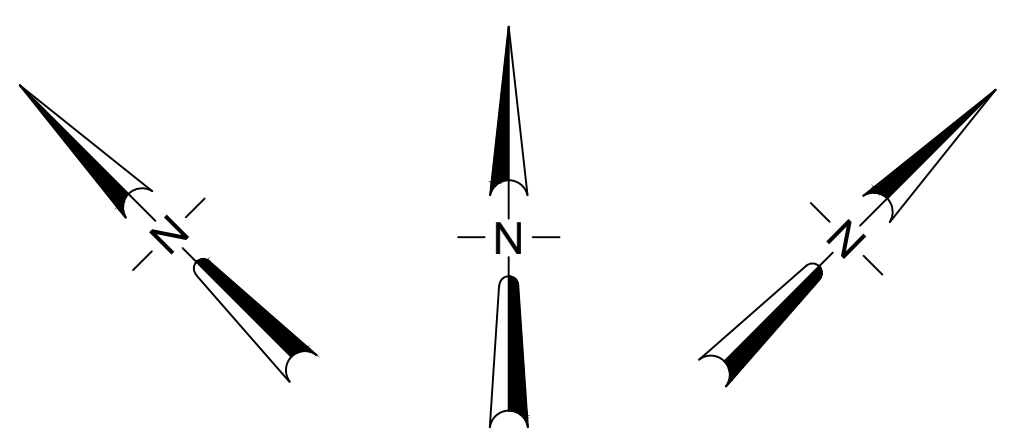
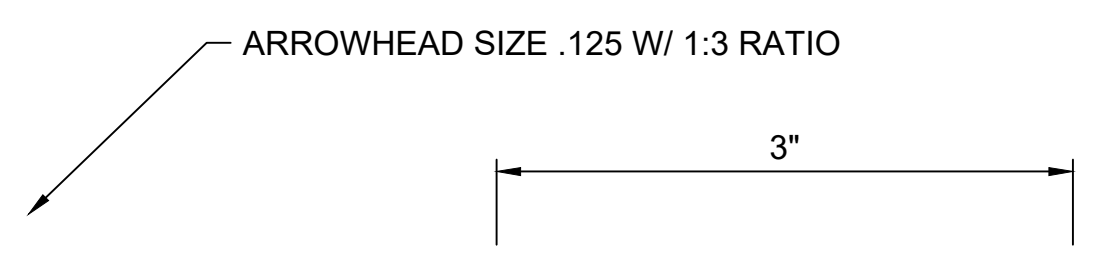


CAN YOU SEE THE AIRPLANES? THE ADJACENT SAMPLES SHOW THREE DIFFERENT LEVELS OF SHADING. SETTINGS FOR VIEWING AND PRINTING DRAWING CONTENT ARE OPTIMIZED WHEN ALL THREE PLANES ARE VISIBLE. THIS GUIDANCE IS PROVIDED FOR REFERENCE ONLY.

NORTH ARROW EXAMPLES:



ARROWHEAD EXAMPLES:



EXAMPLE SHOWN FOR COP PERMITS. IT CAN BE CHANGED TO COH PERMIT OR TURNED OFF WITH A DROP DOWN IN THE UPPER RIGHT CORNER OF THE TITLE BLOCK.

ARCHITECTURAL SCALE BAR EXAMPLES:

SCALE: 6" = 1' - 0" (HALF SCALE) SCALE: 1" = 1' - 0" SCALE: 3/8" = 1' - 0" SCALE: 1/8" = 1' - 0"

SCALE: 3" = 1' - 0" (QUARTER SIZE) SCALE: 3/4" = 1' - 0" SCALE: 1/4" = 1' - 0" SCALE: 3/32" = 1' - 0"

SCALE: 1-1/2" = 1' - 0" SCALE: 1/2" = 1' - 0" SCALE: 3/16" = 1' - 0" SCALE: 1/16" = 1' - 0"

DO NOT EDIT. LEAVE AS SHOWN.

NOTES AND KEY NOTES EXAMPLES:

THE PLURAL FORMAT IS USED FOR BOTH SINGLE AND MULTIPLE NOTES

TEXT SIZE = 0.165" 0.100" SUGGESTED SPACING FOR CLARITY KEY NOTES:

NOTES:

- NOTE 1
- NOTE 2
- NOTE 3 - LINE 1
NOTE 3 - LINE 2
- NOTE 4

TEXT SIZE = 0.125" 0.264" DIA @ FULL SIZE TEXT SIZE = 0.125"

KEY NOTES:

- KEY NOTE 1
- KEY NOTE 2
- KEY NOTE 3 - LINE 1
KEY NOTE 3 - LINE 2
- NOTE 4

SCALES AND SCALE BARS:

- USE N.T.S. FOR NOT TO SCALE. (DO NOT FORGET THE PERIODS AFTER EACH LETTER).
- USE N.T.S. FOR DIAGRAMS, ISOMETRICS, AND ANY PLAN/DETAIL THAT YOU DO NOT WANT TO SHOW THE SCALE (EX. KEY PLANS OR SITE PLANS).
- USE NONE FOR TEXT ONLY SHEETS SUCH AS THE INDEX, NOTES PAGES OR TABLES/SCHEDULES.
- FOR ENGINEERING SCALES, DROP THE TRAILING ZERO (REVIT'S DEFAULT ADDS THEM).
USE SPACES AROUND THE EQUALS SIGN. (EX. 1" = 20')
- PLEASE MAKE SURE THAT THE SCALE BAR MATCHES THE LISTED SCALE.

SHEET NAMING:

- LOCATION (LARGE TO SMALL), THEN TYPE, THEN DISCIPLINE OR SUB-DISCIPLINE.
- ALL TITLES MUST BE UNIQUE, ADD 1, 2, 3 IF NEEDED.
- EXAMPLES:
CONCOURSE C - DEPLANING - FLOOR PLAN - ELECTRICAL
DETAILS 1 - ELECTRICAL
DETAILS 2 - ELECTRICAL

DISCIPLINE NUMBERING:

- SEE PORT STANDARDS APPENDIX 1 FOR ALLOWED DISCIPLINES AND ABBREVIATIONS.
- DISCIPLINE NUMBER FORMAT EXAMPLE: A1.01
(SEE PAGE 9 OF THE CAD STANDARDS FOR AN EXPLANATION OF FORMAT AND DEFINITION OF EACH CHARACTER).

MISC EXAMPLES: (SECTIONS, CALLOUTS, & DETAIL TITLES):

TEXT SIZE = 0.125" 0.625" DIA

0.625" DIA

0.165" TEXT SIZE DO NOT OVER EXTEND THIS LINE.

0.500" DIA 0.125" TEXT SIZE

DETAIL TITLE SCALE: N.T.S.

DETAIL TITLE NOTES:
ALL VIEWS (EVEN SINGLE VIEWS) REQUIRE A DETAIL TITLE.

EXAMPLE OF A CORRECTLY FILLED OUT REVISION BLOCK

0.100" TEXT SIZE @ FULL SIZE

CONSULTANTS INITIALS OF WHO APPROVED REVISION - CAN BE SAME OR DIFFERENT THAN WHO SIGNS THE DRAWING. CONSULTANTS CHOICE

THE DESIGN NUMBER IS ALSO KNOWN AS THE EAN #.

INCLUDE CONSULTANT WITH WHOM THE PORT HAS A CONTRACT ON ALL SHEETS. IF A SUB IS SIGN/STAMPING THE SHEET THEIR INFO MUST APPEAR ON THEIR SHEETS AS WELL. IF A SUB HAS A SUB, BOTH SUBS INFO MUST APPEAR.

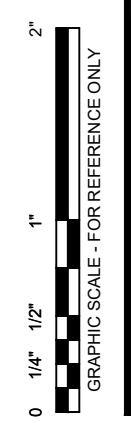
THE PROJECT # IS ALSO KNOWN AS THE PAN OR CHARGE #

LOCATION FOR ARCH OR ENG STAMP. SEE PORT DIGITAL SIGNATURE GUIDANCE FOR REQUIREMENTS (AVAILABLE ON THE PORT WEB SITE).

SET TITLE IS ONE LINE ONLY

SHEET TITLE CAN BE EITHER A SINGLE LINE OR MULTILINE

SHEET NUMBER ONLY DOES NOT INCLUDE TOTAL SHEET COUNT



#	DATE	BY	REVISION	APP
4	8/12/20	CMC	RECORD DRAWING	
3	6/23/20	CMC	REVISED BY CC #4	
2	3/15/20	CMC	AS BID	
1	1/5/20	CMC	REVISED BY ADDENDUM #1	

PORT OF PORTLAND
Possibilities in every direction since 1891.

CONSULTANT INFO GOES HERE. INCLUDE NAME, ADDRESS, PHONE#, LOGO ETC.

2020D001 102000
DESIGN NUMBER PROJECT NUMBER

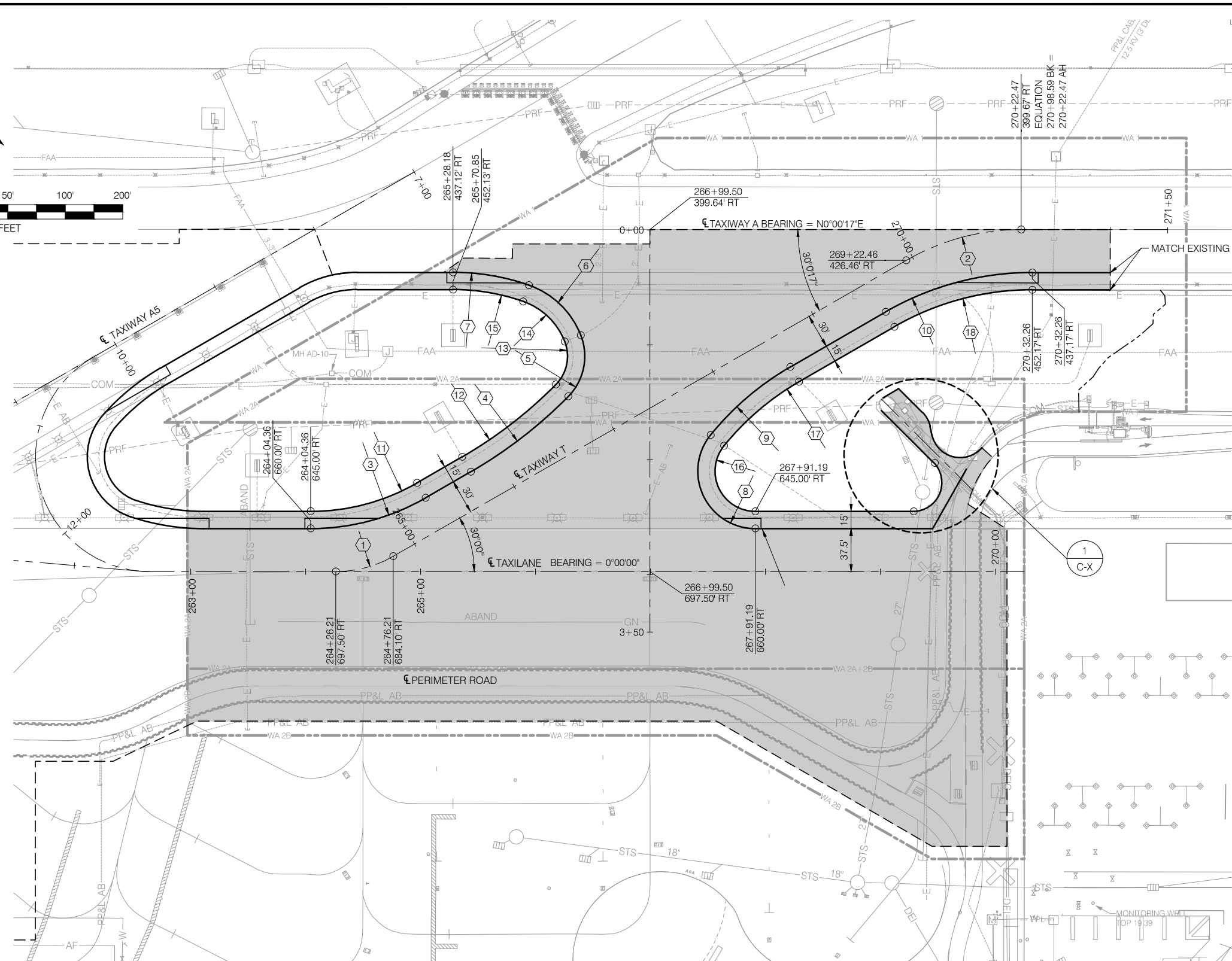
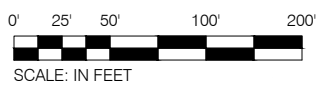
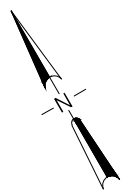
FACILITY

DRAWING SET TITLE

CAD STANDARDS 2020
FAQ SHEET

SUBMITTED BY	MONTGOMERY SCOTT
DESIGN BY	C. CHASE
DRAWN BY	C. CHASE
CHECKED BY	E. WALLS
DATE	AUGUST 2020
SHEET NO.	1
TYPE:	CD
DRAWING NO.	FAC 2020-500
DISC. SHT. NO.	A1.01

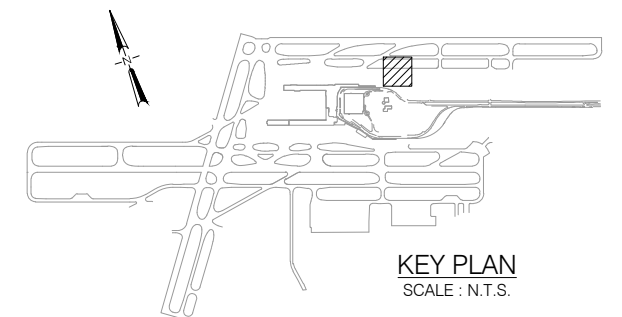
CAN YOU SEE THE AIRPLANES? THE ADJACENT SAMPLES SHOW THREE DIFFERENT LEVELS OF SHADING. SETTINGS FOR VIEWING AND PRINTING DRAWING CONTENT ARE OPTIMIZED WHEN ALL THREE PLANES ARE VISIBLE. THIS GUIDANCE IS PROVIDED FOR REFERENCE ONLY.



CURVE TABLE RUNWAY 10L-28R CENTERLINE COORDINATES						
CURVE	R	DELTA	T	L	RADIUS POINT	
					STATION	OFFSET
①	100.00	30°00'00"	26.79	52.36	264+26.21	597.50' RT.
②	200.00	30°00'17"	53.60	104.74	270+22.46	599.67' RT.
③	200.00	30°00'00"	53.59	104.72	264+04.36	460.00' RT.
④	400.00	15°22'36"	54.00	107.35	263+43.58	264.15' RT.
⑤	50.00	65°58'31"	32.46	57.57	265+92.69	510.01' RT.
⑥	75.00	49°23'36"	34.49	64.66	265+69.40	519.11' RT.
⑦	200.00	19°15'16"	33.93	67.21	265+28.18	637.12' RT.
⑧	50.00	128°58'17"	104.76	112.55	267+91.20	610.00' RT.
⑨	250.00	21°01'43"	46.40	91.75	269+46.69	735.79' RT.
⑩	255.00	30°00'17"	68.34	133.54	270+32.24	692.17' RT.
⑪	185.00	30°00'00"	49.57	96.87	264+04.36	460.00' RT.
⑫	385.00	15°22'36"	51.97	103.32	263+43.58	264.15' RT.
⑬	35.00	65°58'31"	22.72	40.30	265+92.69	510.01' RT.
⑭	60.00	49°23'36"	27.59	51.72	265+69.40	519.11' RT.
⑮	185.00	19°15'16"	31.38	62.17	265+28.18	637.12' RT.
⑯	35.00	128°58'17"	73.33	78.78	267+91.20	610.00' RT.
⑰	235.00	21°01'43"	43.62	86.25	269+46.69	735.79' RT.
⑱	240.00	30°00'17"	64.32	125.68	270+32.24	692.17' RT.

- LEGEND:**
- LIMITS OF PAVING
 - HERBICIDE, SITE GRADING, AND SEEDING LIMITS
 - MILLING LIMITS
 - TAXIWAY SAFETY AREA BOUNDARY
 - WORK AREA LIMITS
 - OBJECT FREE AREA LIMITS

- NOTES:**
- SEE ELECTRICAL DRAWINGS FOR RELOCATION, OR REMOVAL OF EXISTING SIGN PADS AND LOCATION OF NEW SIGN PADS.
 - FOR ADJACENT GEOMETRY TO THE EAST SEE SHEET C-X.



NO.	DATE	BY	REVISIONS	APP'VD	CKD	NO.	DATE	BY	REVISIONS	APP'VD	CKD

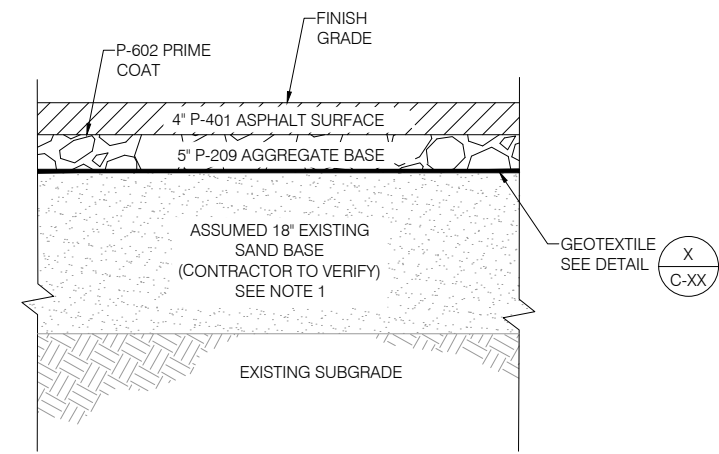


PORT OF PORTLAND
PORTLAND, OREGON

DESIGNED BY D. DESIGNER
 DRAWN BY D. DRAFTER
 CHECKED BY C. CHECKER
 DATE MAY 2013
 SCALE 1" = 50'

PORT OF PORTLAND			
CAD STANDARDS MANUAL EXAMPLE PLAN VIEW DRAWING			
SUBMITTED BY PAT EBERT CAD MANAGER	TYPE CD	DRAWING NO. PDX 2013-XXX	SHEET NO. 1/1
DISC. SHT. NO. C-1		DRAWING SCALE IS REDUCED 50% WHEN SHEET SIZE IS 11" x 17"	

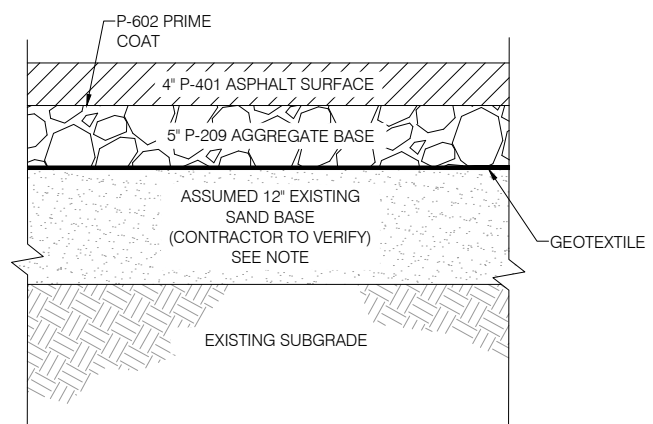
0 1/4 1/2 1 2 3 4 5 6 7 8 9 10
 GRAPHIC SCALE: PART REFERENCE ONLY



NOTE:
 1. CONTRACTOR TO VERIFY DEPTH OF EXISTING SAND BASE PRIOR TO INSTALLING GEOTEXTILE.

1 TAXIWAY E SHOULDER
 SCALE: N.T.S.

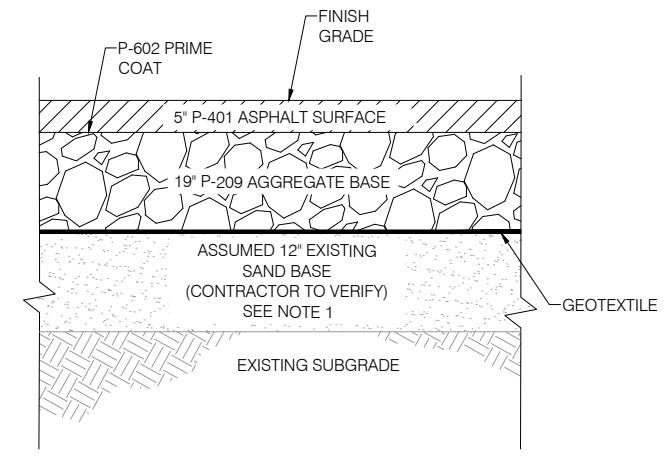
THE NUMBER IS USED ON THIS NOTE AS AN EXAMPLE. IF MULTIPLE INSTANCES OCCUR IN THE DRAWING SET THAT CONTAIN A SINGLE NOTE, USE CONSISTENT FORMATTING. EITHER NUMBER /OR DONT NUMBER, NOT BOTH.



NOTE:
 CONTRACTOR TO VERIFY DEPTH OF EXISTING SAND BASE PRIOR TO INSTALLING GEOTEXTILE.

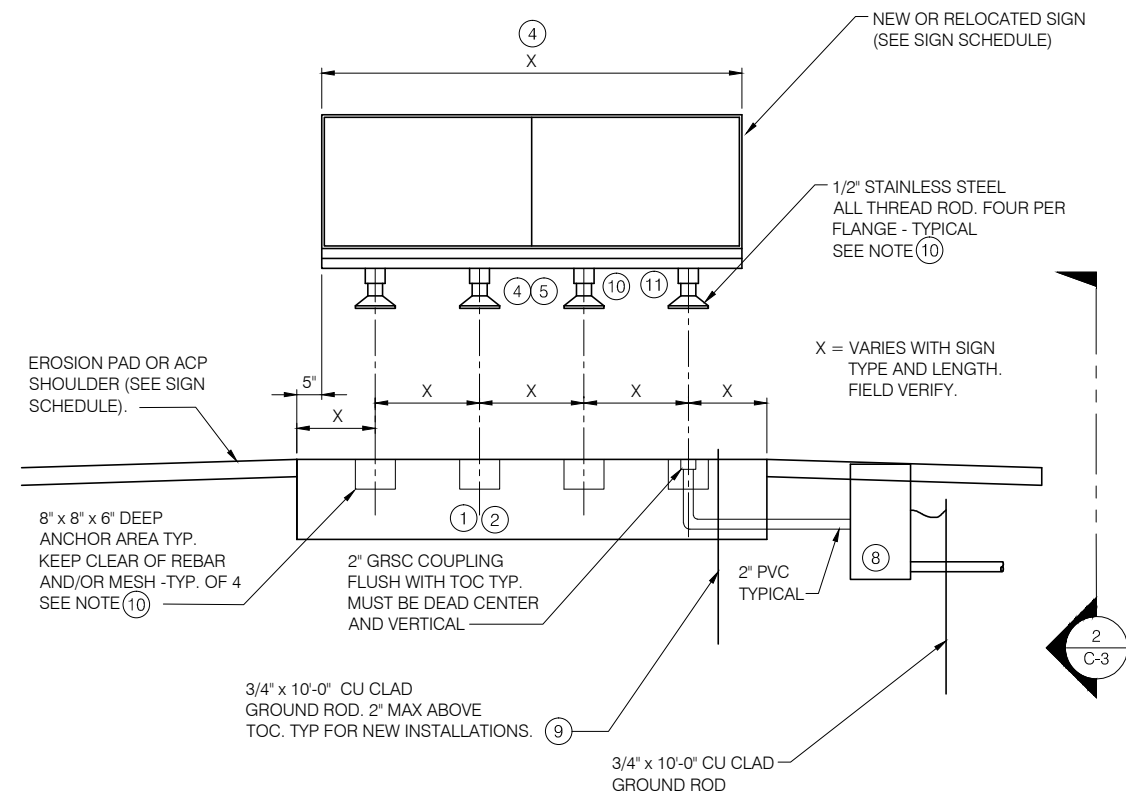
2 TAXILANE ATC SHOULDER
 SCALE: N.T.S.

THE NUMBER IS NOT USED ON THIS NOTE AS AN EXAMPLE. IF MULTIPLE INSTANCES OCCUR IN THE DRAWING SET THAT CONTAIN A SINGLE NOTE, USE CONSISTENT FORMATTING. EITHER NUMBER /OR DONT NUMBER, NOT BOTH.

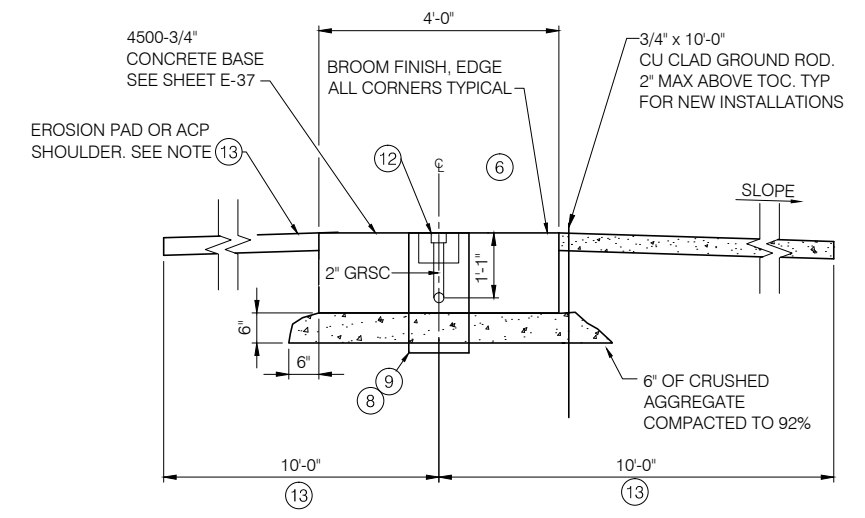


NOTES:
 1. CONTRACTOR TO VERIFY DEPTH OF EXISTING SAND BASE PRIOR TO INSTALLING GEOTEXTILE.
 2. APPLY 5\"/>

3 TAXIWAY F SHOULDER
 SCALE: N.T.S.



4 FOUNDATION FOR SINGLE AIRCRAFT GUIDANCE
 SCALE: N.T.S.



5 TYPICAL SIGN SECTION
 SCALE: N.T.S.

KEY NOTES:

- 1 FOUNDATION DETAILS ARE PROVIDED FOR CONTRACTOR INFORMATION ONLY. THEY REPRESENT THE ESTABLISHED PORT STANDARD DESIGNS FOR THE AIRCRAFT GUIDANCE SIGN TYPE IN USE AT PORTLAND INTERNATIONAL AIRPORT.
- 2 CAREFULLY MEASURE EACH EXISTING SIGN TO BE RELOCATED AND APPLY THOSE DIMENSIONS TO DETAIL 1. AS REQUIRED, THE WIDTH AND DEPTH SHOWN ARE MINIMUMS. THE LENGTH MUST BE DETERMINED USING THE SIGN LENGTH PLUS THE DIMENSION GIVEN.
- 3 NEW SIGNS TO BE INSTALLED SHOULD BE CONSIDERED TYPICAL AS SHOWN IN DETAIL 1. FOR THE PURPOSES OF FOUNDATION AND EROSION PAD SIZING, MEASUREMENT ADJUSTMENTS MAY BE REQUIRED DEPENDING UPON SIGN MANUFACTURER.
- 4 CONFIRM THE ACTUAL MEASUREMENTS AND CONFIGURATION OF EACH NEW SIGN AND APPLY THOSE DIMENSIONS TO DETAIL 1. THE WIDTH AND DEPTH SHOWN ARE MINIMUMS. FOR EXAMPLE, THE FOUNDATION LENGTH WILL BE DETERMINED USING THE SIGN LENGTH PLUS THE DIMENSIONS GIVEN. THE CONCRETE FOUNDATIONS SHALL EXTEND BOTH LONGITUDINAL DIRECTIONS 5-INCHES BEYOND THE SIGN LENGTH.
- 5 EACH FOUNDATION SHALL HAVE A MINIMUM OF TWO 3/4 INCH, HEAVY-DUTY, EMBEDDED LIFTING INSERTS RATED BY A FACTOR OF TWO TO THE CALCULATED LOAD AND INSTALLED FOR OPTIMUM WEIGHT DISTRIBUTION.
- 6 SURVEY AND STAKE EACH SIGN LOCATION. THE DIMENSIONS PROVIDED IDENTIFY THE FOUNDATION CENTER AT THE END OF THE FOUNDATION NEAREST THE ACTIVE PAVEMENT. UNLESS OTHERWISE INDICATED, EACH SIGN CENTERLINE SHALL BE SET PERPENDICULAR TO THE TAXIWAY OR RUNWAY CENTERLINE WITH THE FOUNDATION NO FURTHER THAN 55 FEET SETBACK FROM THE PAINTED STRUCTURAL EDGE OF THE ACTIVE PAVEMENT. EXCEPTION: DISTANCE REMAINING SIGNS WILL BE 75 FEET.
- 7 PRIOR TO EXCAVATION AT EACH SIGN LOCATION, THE CONTRACTOR SHALL CAREFULLY OBSERVE AND NOTE THE EXISTING SITE ELEVATION AND GRADE. MAINTAINING THE EXISTING DRAINAGE, ESTABLISH THE FOUNDATION ELEVATION BASED UPON THE EROSION PAD STARTING FLUSH WITH THE TOP OF FOUNDATION AND SLOPING A MAXIMUM OF 3% IN ALL DIRECTIONS. THE EDGES OF THE EROSION PAD SHALL FINISH 1-1/2 INCH ABOVE GRADE TO ALLOW FOR TURF BUILD-UP.
- 8 NEW OR RELOCATED L-867 JUNCTION CAN SHALL BE LOCATED CLEAR OF THE FOUNDATION AND FLUSH WITH THE EROSION PAD FINISH GRADE. WHEREVER POSSIBLE, IT SHALL BE PLACED NEAR THE END OF THE SIGN CLOSEST TO THE ACTIVE PAVEMENT. THE BASE CAN SHALL BE SET AT THE HIGHEST ADJACENT PAVEMENT ELEVATION. NO LOW SPOTS IN ASPHALT WILL BE ALLOWED. CONVENIENCE WILL NOT BE A FACTOR IN PLACING JUNCTION CANS OR ROUTING DUCTS.
- 9 EACH JUNCTION CAN, THROUGH AN EXTERNAL GROUND LUG ON THE JUNCTION CAN, SHALL BE GROUNDED TO A 3/4\"/>

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										PORT OF PORTLAND PORTLAND, OREGON										DESIGNED BY <u>D. DESIGNER</u> DRAWN BY <u>D. DRAFTER</u> CHECKED BY <u>C. CHECKER</u> DATE <u>MAY 2013</u> SCALE <u>N.T.S.</u>										PORTLAND INTERNATIONAL AIRPORT CAD STANDARDS MANUAL EXAMPLE DETAIL DRAWING																																							
2012DXXX <small>DESIGN NUMBER</small>										101245 <small>PROJECT NUMBER</small>										SUBMITTED BY <u>PAT EBERT</u> <small>CAD MANAGER</small>										TYPE <u>CD</u>										DRAWING NO. <u>PDX 2013-XXX</u>										SHEET NO. <u>3/3</u>										DISC. SHT. NO. <u>C-3</u>									
<small>NO. DATE BY REVISIONS APP'VD CKD</small>										<small>NO. DATE BY REVISIONS APP'VD CKD</small>										<small>PRINTED: 6/30/2015 1:44:21 PM EBERTP x:\pop_standards\cad_standards - current\appendix 3.dwg</small>										<small>DRAWING SCALE IS REDUCED 50% WHEN SHEET SIZE IS 11" x 17"</small>																																							